



Cycle Segregation Unit

Why Cycling?

Parliament has endorsed targets for 10% of all journeys being made by bike by 2025, with significant new funding being made available to support cycling in the UK. The latest figures show that for all the short journeys made by car - 11 per cent are under a mile, 29 per cent are between one and two miles, and the remainder 60 per cent are between two and five miles.

While no one can argue with the social, economic and health benefits that cycling offers, there is no doubt that the environmental infrastructure of Britain's public realm was never developed with the cyclist in mind, and at best it has only been an afterthought.

Great cities give their citizens room for interaction and nobody interacts by driving from A to B at speed.

NIELS HOE CEO OF HOE360 CONSULTING

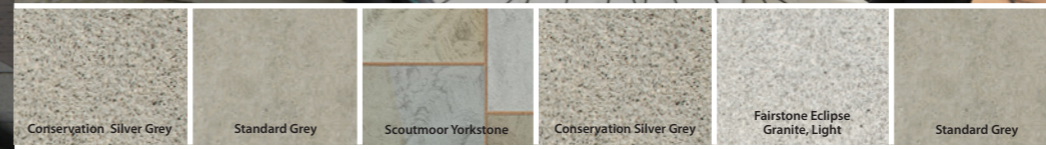


Traffic Calming

Traffic Calming

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www.marshalls.co.uk/commercial



Cycle Segregation Unit

Kerb Finish

Infill Options

The Cycle Segregation unit by Marshalls is a versatile concrete heavy cycling segregation product. It is used to create a protective barrier between cyclists and vehicles, ensuring safety for experienced and beginner cyclists alike.

The unit is available in either a Natural or Conservation kerb finish with a choice of Marshalls Granite or Scoutmoor Yorkstone Setts or Concrete infill providing various options to ensure its integration into the landscape.

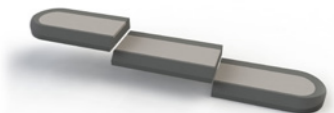
*PLEASE NOTE, CONSERVATION SILVER GREY CANNOT BE COMBINED WITH STANDARD GREY.

Marshalls uses MTEC concrete which is self compacted, meaning that the material does not need to be artificially, mechanically or hydraulically pressed. The improvement in the water-cement ratio makes the concrete stronger, improving its compressive and flexural strength and making it a more robust material than standard concrete. MTEC has a lower water absorption in a smooth grey finish, keeping the product cleaner for longer and minimising maintenance costs. As a material, it is also highly resistant to algae and de-icing salts compared to standard concrete.

300mm width
Cycle segregation unit



500mm width
Cycle segregation unit





Cycle Segregation Unit

The segregation unit uses bullnosed and 45 degree splayed kerb profiles to ensure a safe passage for the cyclist. The outside bullnosed profile is design to stop vehicles mounting the unit to potentially endanger the cyclist. A 45 degree splayed kerb on the inside means cyclists of all abilities can use the cycleway without fear of colliding with a steep kerb. Marshalls caters for both turning and straight road segregation pieces. Send us your road map and we will create a cycle segregation piece that will meet your requirements.

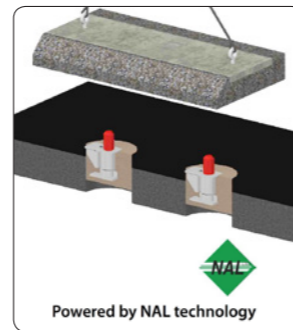
The unit is demountable and utilises NAL sockets, meaning it can be disassembled and relocated when required.

NAL Sockets are available in a standard or shallow depth to help where excavation might be an issue.

All units are pre-formed in factory controlled conditions, ensuring design accuracy and specification validity.

- The kerbs comply with BS EN:1340
- M16 lifting eyes are provided.

For datasheets and installation details, contact us on 0370 600 24 25

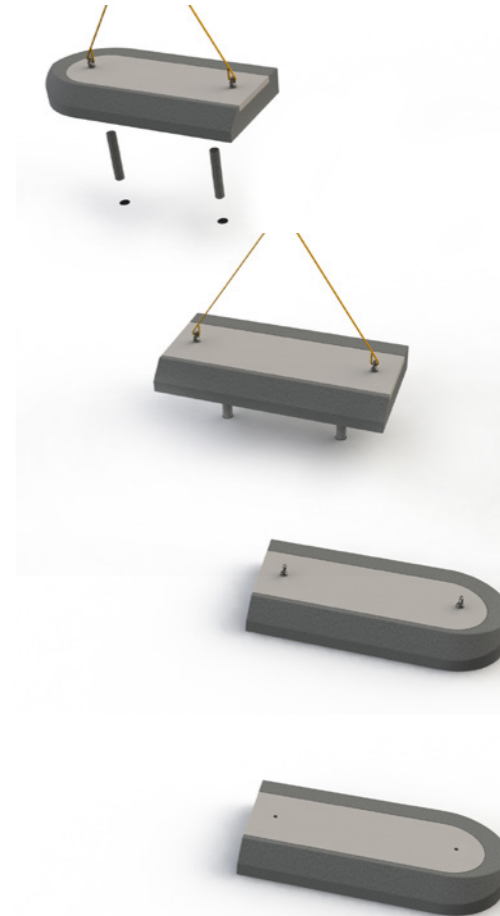


Installation Instructions

For the complete installation of NAL sockets please refer to full instructions by NAL before work commences.

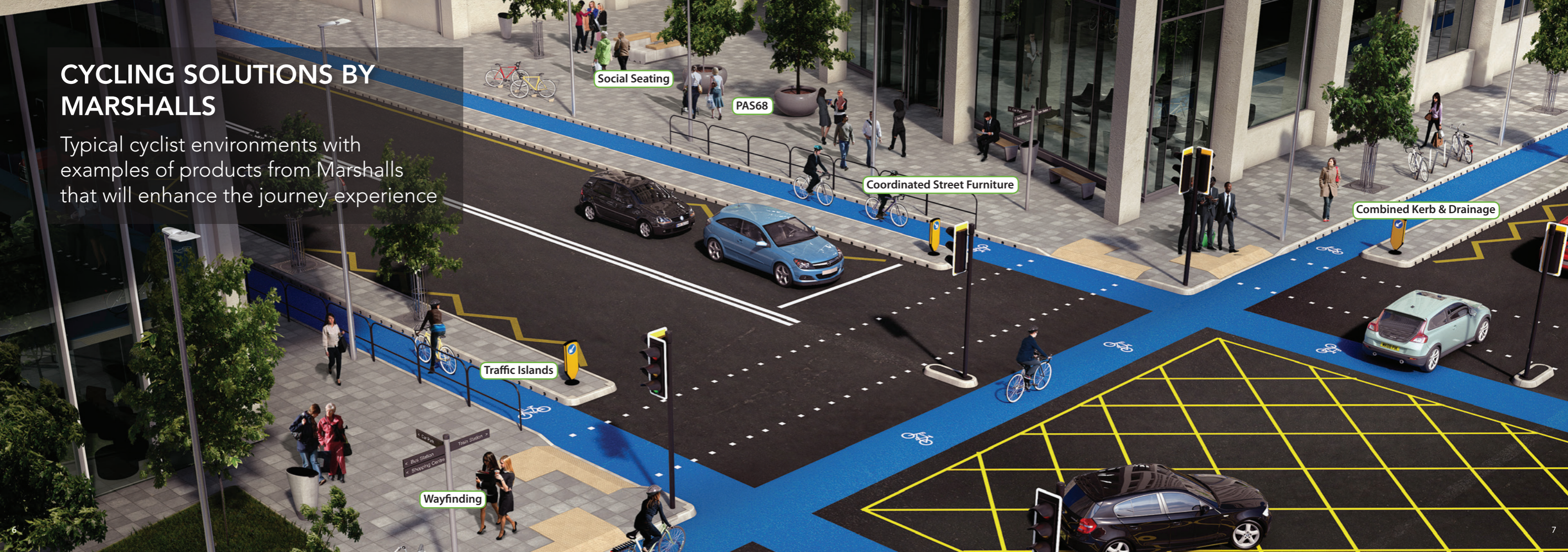
1. Excavate holes to accept NAL sockets. Install sockets and then place pins in the sockets as per the NAL installation drawing.
2. Insert M16 lifting eyes into the appropriate sockets as per drawing. Ensure that the bolts are fully tightened and that there is no movement.
3. Insert sling through lifting eyes, making sure it is fully secure. Slings equipment should be suitable for lifting the required load.
4. Lift unit above the NAL socket and lower into position carefully, ensuring that the locating pin is fully engaged into the segregation unit opening.
5. Once in position, safely disengage the slings and lifting eyes.

This installation guide is recommended for flat level surfaces. If the surface is not flat, it is advisable to level it, either by adding asphalt or concrete to the surface.



CYCLING SOLUTIONS BY MARSHALLS

Typical cyclist environments with examples of products from Marshalls that will enhance the journey experience



Social Seating

PAS68

Coordinated Street Furniture

Combined Kerb & Drainage

Traffic Islands

Wayfinding



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